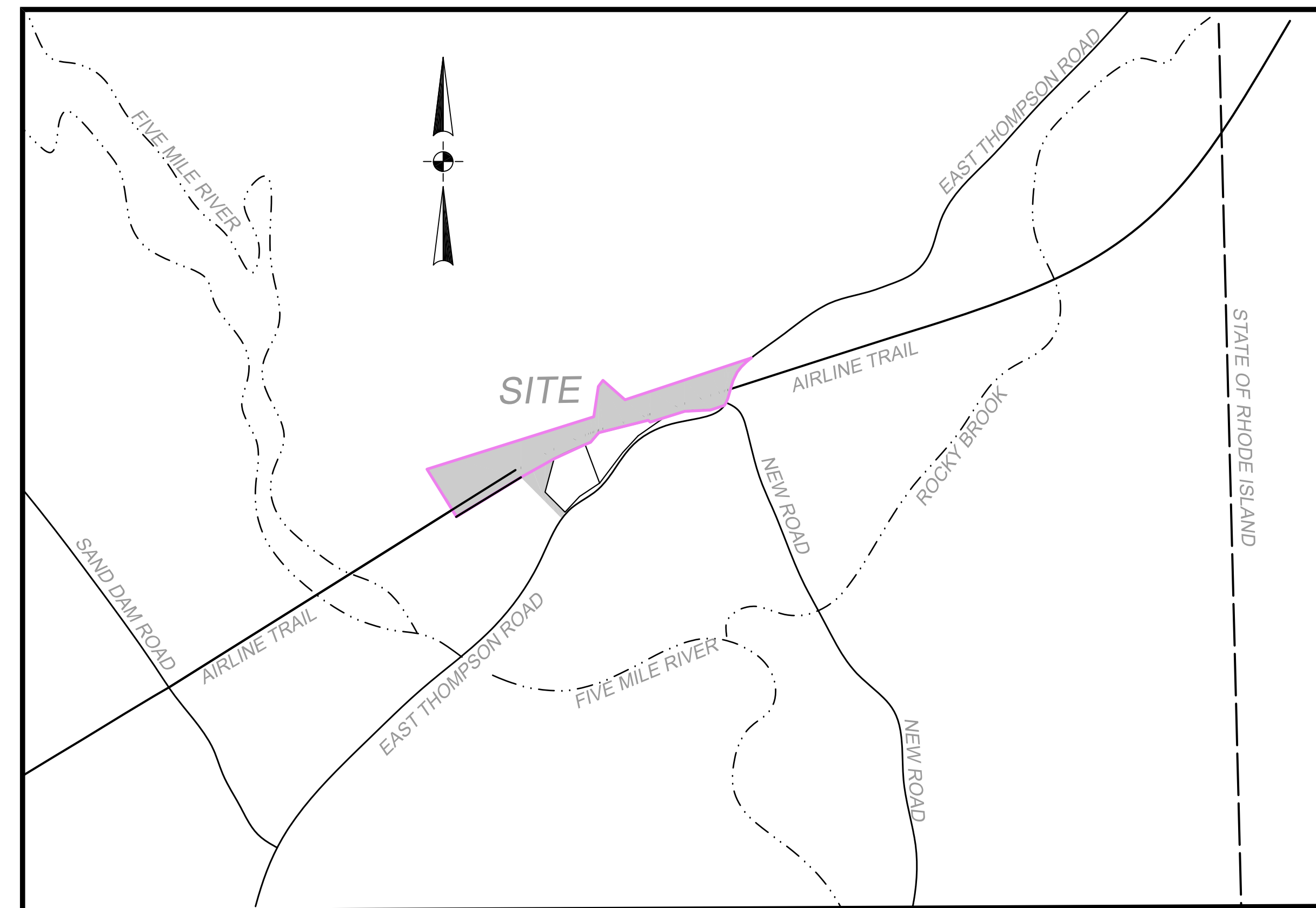


TRAIN WRECK PARK
EAST THOMPSON RD, THOMPSON CT
OWNER/APPLICANT: TOWN OF THOMPSON
DATED: MARCH 26, 2024

INDEX OF DRAWINGS

- | | |
|---|-------------------------------------|
| 1 | COVER SHEET AND LOCATION MAP |
| 2 | OVERALL SITE PLAN |
| 3 | PARKING LOT PLAN |
| 4 | TURNTABLE AREA PLAN |
| 5 | NOTES AND DETAILS |
| 6 | NOTES AND DETAILS |
| 7 | CT DOT SIGN PLACEMENT TR-1208_01 |
| 8 | CT DOT METAL SIGN POST TR-1208_02 |
| 9 | CT DOT PAVEMENT MARKINGS TR-1210_04 |



LOCATION MAP

1" = 500'

THIS PROJECT FUNDED, IN PART, BY THE STATE OF CONNECTICUT DEPARTMENT OF ENERGY AND ENVIRONMENTAL PROTECTION "CONNECTICUT RECREATIONAL TRAILS GRANT PROGRAM".

J & D **CIVIL
ENGINEERS, LLC**
401 RAVENELLE ROAD
THOMPSON, CT 06255
JDCIVILENGINEERS.COM
860-923-2920



BID ITEM #1 - TURNTABLE
INTERPRETIVE AREA LIMIT
OF CONSTRUCTION

BID ITEM #2 - PARKING
LOT AREA LIMIT OF
CONSTRUCTION

LOT 68
N/F THOMPSON ROD
AND GUN CLUB

LOT 67
N/F THOMPSON ROD
AND GUN CLUB

LOT 66
N/F BOYDEN

LOT 7
N/F ANTONUCCI

LOT 6
N/F ELLIOTT

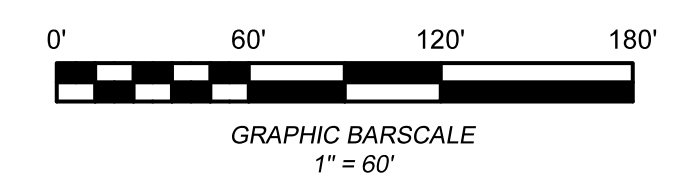
LOT 5
N/F COSMA

AIRLINE TRAIL STATE PARK
N/F STATE OF CONNECTICUT

AIRLINE TRAIL STATE PARK
N/F STATE OF CONNECTICUT

EAST THOMPSON ROAD

NEW ROAD



LEGEND

- EXISTING PROPERTY LINE
- ABUTTING PROPERTY LINE
- EDGE OF EASEMENT
- STONE WALL
- UTILITIES
- TREELINE

OVERALL SITE PLAN
PREPARED FOR
TOWN OF THOMPSON
EAST THOMPSON ROAD - THOMPSON, CT

J&D CIVIL
ENGINEERS, LLC
401 RAVENELLE ROAD
N. GROSVENORDALE, CT 06255
860-923-2920

DESIGNED: APS CHECKED: JAB	REVISIONS:
JOB NO: 23239 SCALE: 1" = 60'	DATE: MARCH 26, 2024 SHEET: 2



THE CONTRACTOR SHALL STILL BE RESPONSIBLE FOR
INSTALLING THE THREE PAVED APRONS EVEN IF
THE TOWN DECIDES TO SELECT THE "ELIMINATE
BITUMINOUS CONCRETE" BID ALTERNATE.

BIKE RACK AND BIKE REPAIR STATION NOTES

THE TOWN INTENDS TO PURCHASE THE BIKE RACK (THE PARK CATALOG 9 WAVE
RACK) AND BIKE REPAIR STATION (THE PARK CATALOG DELUXE PUBLIC WORK
STAND) AND WILL DELIVER IT TO THE SITE FOR THE CONTRACTOR TO INSTALL.

1. BASE BID SHALL INCLUDE INSTALLING THE STRUCTURES IN CONCRETE PIERS AS
PER MANUFACTURER'S REQUIREMENTS. PADS AROUND STRUCTURES SHALL BE
6" PROCESSED GRAVEL.
2. CONCRETE SLAB BID ADD ALTERNATE: INSTALL CONCRETE SLAB AROUND
CONCRETE PIERS TO DIMENSIONS SHOWN ON PLAN AND AS PER THE
TYPICAL SECTION.

APPROX. CLEARING LIMITS
AREA= 40,000± SF
NOTE: MATURE TREES IN GOOD
CONDITION NEAR THE EDGES OF
THE CLEARING LIMITS SHALL BE
SAVED IF POSSIBLE.

INSTALL PERMANENT CT DEEP
RECREATIONAL TRAILS PROGRAM
GRANT FUNDING SIGN

RELOCATED LOCATION
EXISTING KIOSK

RELOCATED LOCATION
SIGN "IN THE AIR
AND OFF THE TRACKS"

INSTALL NEW YELLOW GATE SIMILAR TO
EXISTING GATE. EXACT LOCATION TBD BY
TOWN. BOULDERS WITH 36" GAPS SHALL BE
PLACE BEYOND GATE ON BOTH SIDES TO
DISCOURAGE ATV TRAFFIC.

AIRLINE TRAIL STATE PARK
N/F STATE OF CONNECTICUT

EAST THOMPSON ROAD

24" X 24" TRAILHEAD PARKING
WITH LEFT ARROW AND AHEAD
SIGNS.

LOT 66
N/F JESSICA BOYDEN

LOT 67
N/F THOMPSON ROD
AND GUN CLUB

AIRLINE TRAIL
STATE PARK
N/F STATE OF CONNECTICUT

TRAIL CROSSING AHEAD
SIGNS MUTCD W11-15, W11-15P,
AND W16-9P

ADD WHITE SHOULDER
LINES APPROXIMATELY
238' LONG AS INDICATED.
LANE WIDTH 13' - 14' AS
SHOWN.

PROPOSED RELOCATED AIRLINE
TRAIL AROUND PARKING LOT

ROW OF SCREENING VEGETATION
(INCLUDE IN BASE BID)

TRAIL CROSSING SIGNS
MUTCD W11-15, W11-15P,
AND W16-7P

CROSS WALK

TRAIL CROSSING SIGNS
MUTCD W11-15, W11-15P,
AND W16-7P

REMOVE EXISTING
CROSSING SIGNS

TYPE CL CB
FRAME = 469.60
INV IN = 465.25
INV OUT = 463.05
PROP. INV. = 466.0

GRADE SHOULDERS
TO DRAIN TO CB

TYPE C CATCH BASIN
FRAME = 468.91
INV IN = 462.61

TRAIL CROSSING AHEAD
SIGNS MUTCD W11-15, W11-15P,
AND W16-9P

SIGN "BUS STOP
AHEAD"

SURVEY NOTES

1. THIS MAP HAS BEEN PREPARED PURSUANT TO THE REGULATIONS OF
CONNECTICUT STATE AGENCIES SECTIONS 20-300b-1 THROUGH 20-300b-20 AND
THE "STANDARD FOR SURVEYS AND MAPS IN THE STATE OF CONNECTICUT" AS
ADOPTED BY THE CONNECTICUT ASSOCIATION OF LAND SURVEYORS, INC. ON
SEPTEMBER 26, 1996.

SURVEY TYPE: TOPOGRAPHIC AND GENERAL LOCATION

BOUNDARY DETERMINATION CATEGORY: NONE

HORIZONTAL ACCURACY: CLASS B
VERTICAL ACCURACY: CLASS T2

PURPOSE: SITE DESIGN

PROPERTY LINES DO NOT EXPRESS A BOUNDARY OPINION

2. REFERENCE PLANS:
- (A) RIGHT OF WAY AND TRACK MAP FOR THE NEW YORK, NEW HAVEN, AND
HARTFORD RAILROAD COMPANY, STATION 2748+40.4 TO STATION 2801+20.
TOWN OF THOMPSON, STATE OF CONNECTICUT. SCALE 1" = 100', DATE JUNE 30,
1915. MAP NUMBER V54.60/1
 - (B) RIGHT OF WAY AND TRACK MAP FOR THE NEW YORK, NEW HAVEN, AND
HARTFORD RAILROAD COMPANY, STATION 0+0 TO STATION 26+40. TOWN OF
THOMPSON, STATE OF CONNECTICUT. SCALE 1" = 50'. DATE JUNE 30, 1915. MAP
NUMBER V4.50/1
 - (C) RIGHT OF WAY AND TRACK MAP FOR THE NEW YORK, NEW HAVEN, AND
HARTFORD RAILROAD COMPANY, ABANDONMENT MAPS, 8 SHEETS. ON FILE
WITH THE TOWN CLERK AS MAP #158.
 - (D) PLAN OF LAND TO BE TAKEN FROM HORETTA N. SOPER, AT THOMPSON,
WINDHAM COUNTY, CONNECTICUT. DATED NOVEMBER 6, 1884. ON FILE WITH
THE TOWN CLERK ON VOLUME 29, PAGE 88-89.
 - (E) PLAN PREPARED FOR EDWARD MASTERSON JR, JOSEPH J. & ANNE M.
SELMECKI, JOHN TRULL & ELEANOR CRAVER, EAST THOMPSON ROAD (REAR)
EAST THOMPSON, CT, PLAN PREPARED BY EMIL W. LUCEK ASSOCIATES, DATED
1/31/02, REVISED 6/13/02 ON FILE WITH THE TOWN CLERK AS MAP #1422-2

TO MY KNOWLEDGE AND BELIEF, THIS MAP IS SUBSTANTIALLY CORRECT AS
NOTED HEREON.

DENNIS R. BLANCHETTE DATE 12/07/24 LICENSE #

THIS MAP IS NOT VALID WITHOUT A LIVE SIGNATURE
© 2024 J&D CIVIL ENGINEERS, LLC

LEGEND

- EXISTING PROPERTY LINE
- ABUTTING PROPERTY LINE
- BUILDING SETBACK
- STONE WALL
- TREELINE
- MAJOR CONTOUR LINE
- MINOR CONTOUR LINE
- PROPOSED CONTOUR
- EROSION CONTROL DEVICES
- UTILITIES
- EXISTING DRAINAGE PIPE
- PROPOSED DRAINAGE PIPE
- ANGLE POINT
- UTIL POLE
- SIGN
- CATCH BASIN

PARKING LOT PLAN
PREPARED FOR
TOWN OF THOMPSON
EAST THOMPSON ROAD - THOMPSON, CT

J&D CIVIL
ENGINEERS, LLC
401 RAVENELLE ROAD
N. GROSVENORDALE, CT 06255
860-923-2920

DESIGNED: APS
CHECKED: JAB

REVISIONS:

JOB NO: 23239

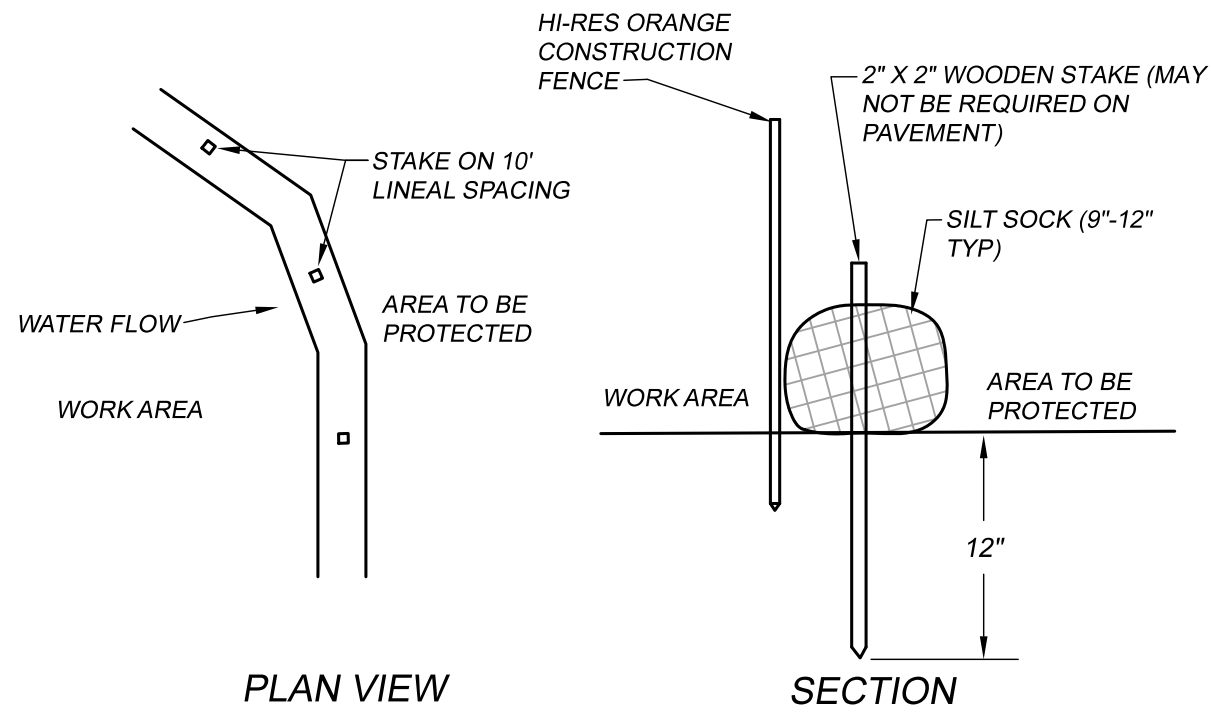
DATE: MARCH 26, 2024

SCALE: 1" = 20'

SHEET: 3



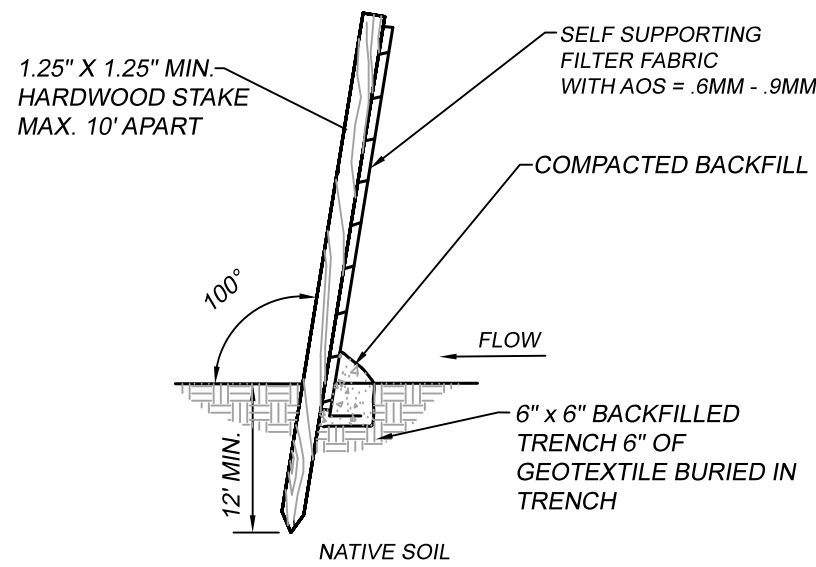
SIGN SCHEDULE



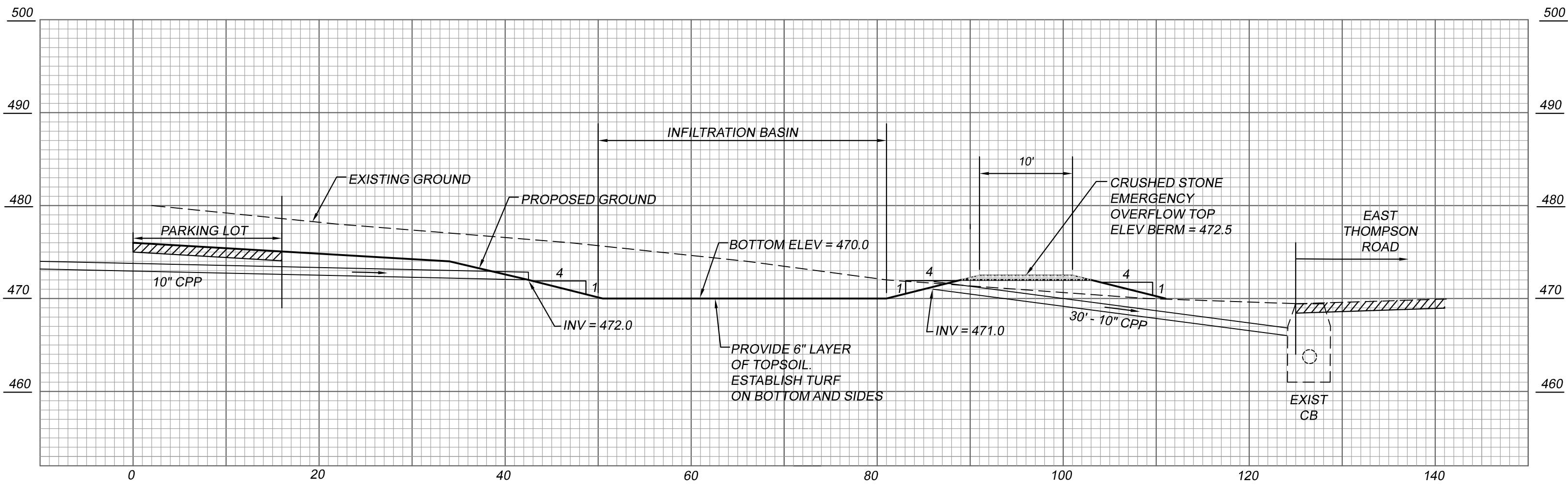
NOTES

1. SILT SOCK MANUFACTURER SHALL BE SILT SOXX OR ENGINEER APPROVED EQUAL
2. ALL MATERIAL TO MEET MANUFACTURER'S SPECIFICATIONS
3. SEDIMENT SILT SOCK TO BE FILLED WITH LEAF COMPOST AND/OR WOODY MULCH PER MANUFACTURER'S REQUIREMENTS
4. FOLLOWING CONSTRUCTION AND SITE STABILIZATION, COMPOST MATERIAL SHALL BE REMOVED OR DISPERSED ON SITE, AS APPROVED BY THE ENGINEER.

SILT SOCK DETAIL
NOT TO SCALE



SILT FENCE INSTALLATION
NOT TO SCALE

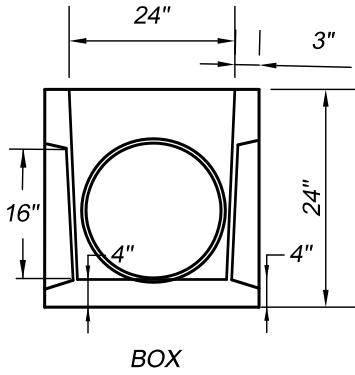
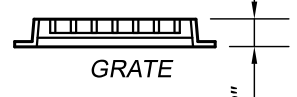
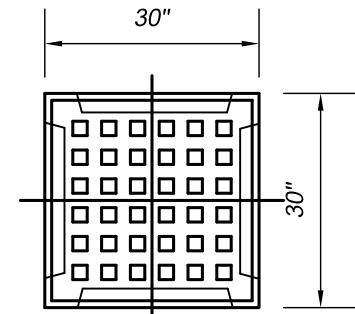


STORM WATER INFILTRATION BASIN SECTION

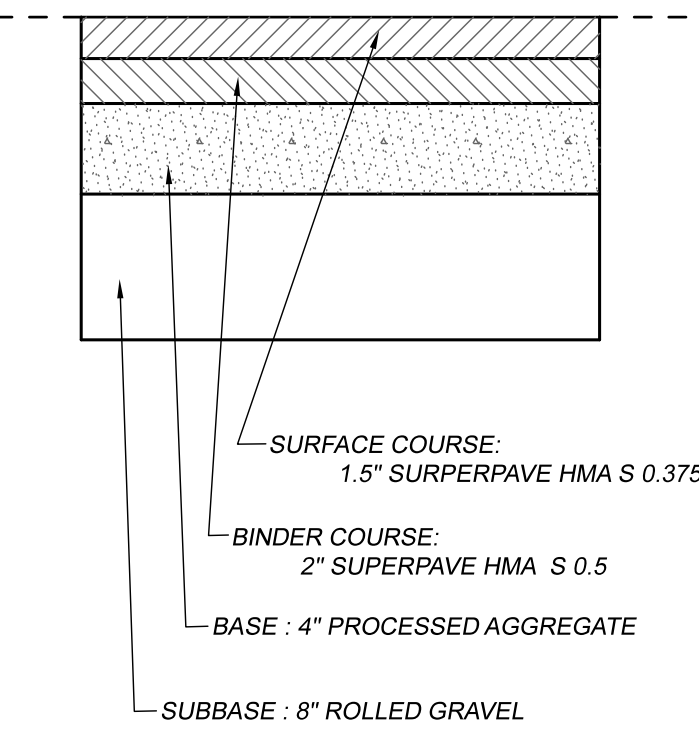
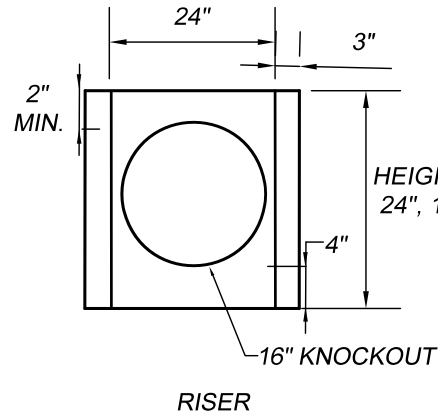
1" = 10'

SIGN CATALOG NUMBER	MUTCD NUMBER	DESCRIPTION	SIZE	QUANTITY
31-0532	R1-1	STOP	18"	2
31-0552	R1-1	STOP	30"	1
31-0512		YIELD HERE TO PEDESTRIANS	36" X 36"	2
41-4839	W11-15P	TRAIL X-ING	18" X 24"	4
31-5004	RS	RED RETROREFLECTIVE STRIPS (STOP)	4" X 72"	1
41-2617	W16-7P	DOWNWARD DIAGONAL LEFT ARROW	24" X 12"	2
41-4828	W11-15	BICYCLE PEDESTRIAN SIGN	30" X 30"	4
41-5001	RS	RETROREFLECTIVE STRIP	4" X 48"	4
41-6126	W16-9P	AHEAD	24" X 12"	3
		TRAILHEAD PARKING WITH LEFT ARROW	24" X 24"	1
31-0659	R7	HANDICAPPED PARKING SIGN	18" X 24"	2
31-0648	R7-8P	VAN ACCESSIBLE	18" X 9"	2
		TRAILER PARKING ONLY	18" X 24"	3

- DESIGN NOTES
- 1.) 16" DIAMETER KNOCKOUT - 4 PLACES
 - 2.) TYPE II CEMENT ASTM C150-81
 - 3.) CONCRETE STRENGTH 5000 PSI MIN. 28 DAYS
 - 4.) CAN WITHSTAND MEDIUM-DUTY TRAFFIC
 - 5.) GRATE IS BICYCLE SAFE



24" x 24" YARD BOX
NOT TO SCALE



MATERIAL SHALL MEET CT DOT FORM 818, AS AMENDED

TYPICAL PARKING LOT SECTION

NOT TO SCALE

SEQUENCE OF CONSTRUCTION :

CONSTRUCTION ACTIVITIES SHALL PROCEED IN ACCORDANCE WITH THE FOLLOWING SEQUENCE.

1. HOLD PRE-CONSTRUCTION MEETING WITH OWNER, SITE CONTRACTOR, AND DESIGN ENGINEER.
2. EXCAVATING CONTRACTOR SHALL NOTIFY CALL BEFORE YOU DIG AS REQUIRED, AND IS RESPONSIBLE FOR VERIFYING THE LOCATION OF ALL BURIED UTILITIES PRIOR TO COMMENCING CONSTRUCTION.
3. INSTALL COMPOST FILTER SOCK OR SILT FENCE ALONG THE DOWNSLOPE SIDE OF CONSTRUCTION ACTIVITIES AS SHOWN ON THE DRAWINGS.
4. ROUGH GRADING.
5. DEMOLITION OF ISLANDS, SIGNS, TREES, EXISTING DRAINAGE STRUCTURES, ETC.
6. FULL DEPTH PAVEMENT RECLAMATION OF EXISTING PARKING LOT.
7. INSTALL NEW DRAINAGE.
8. INSTALL UNDERGROUND ELECTRIC AND LIGHT POLE BASES.
9. PAVING.
10. PAVEMENT MARKINGS AND SIGNS.
11. LANDSCAPING
12. FINAL SITE STABILIZATION AND CLEANUP
13. AFTER PERMANENT STABILIZATION OF SITE (I.E. A MINIMUM UNIFORM 70% PERENNIAL VEGETATIVE COVER, WITH A DENSITY CAPABLE OF RESISTING ACCELERATED EROSION AND SEDIMENTATION) HAS BEEN ACHIEVED, THE TEMPORARY EROSION AND SEDIMENTATION CONTROLS MUST BE REMOVED. AREAS DISTURBED DURING THE REMOVAL OF THE CONTROLS SHALL BE RESTABILIZED. PERMANENT STORMWATER FEATURES SHOULD BE CLEANOUT OUT AS NEEDED UPON FINAL STABILIZATION OF THE SITE.

GENERAL SEEDING NOTES

1. TEMPORARY SEEDING NOTES - SITE PREPARATION: APPLY 1-2 TON /ACRE AGRICULTURAL GRADE LIMESTONE AND 10-10-10 FERTILIZER AT A RATE OF 300 LBS./ACRE AND WORK IN WHERE POSSIBLE. REFER TO FIGURE TS-2 IN THE 2023 CT GUIDELINES FOR SOIL EROSION AND SEDIMENT CONTROL FOR APPROPRIATE SEEDING MIXES AND RATES. MULCH SEEDED AREAS IMMEDIATELY AFTER SEEDING.
2. PERMANENT SEEDING NOTES - SITE PREPARATION: GRADE AS NECESSARY TO BRING THE SUBGRADE TO A TRUE, SMOOTH SLOPE PARALLEL TO AND SIX INCHES BELOW FINISHED GRADE. PLACE TOPSOIL OVER SPECIFIED AREAS TO A DEPTH SUFFICIENTLY GREATER THAN SIX INCHES SO THAT AFTER SETTLEMENT AND LIGHT ROLLING THE COMPLETE WORK WILL CONFORM TO LINES, GRADES AND ELEVATIONS SHOWN.
3. APPLY 4 TONS/ACRE AGRICULTURAL GRADE LIMESTONE AND 10-10-10 FERTILIZER AT A RATE OF 300 LBS/AC OR AS PER SOIL TEST. LIMESTONE AND FERTILIZER MAY NOT BE REQUIRED IN AGRICULTURAL FIELDS.
4. FERTILIZER AND AGRICULTURAL LIMESTONE SHALL BE THOROUGHLY INCORPORATED INTO THE SOIL BY ROTOTILLING OR OTHER METHOD TO A MINIMUM DEPTH OF FOUR INCHES. THE ENTIRE SURFACE SHALL BE DONE IN TWO SEPARATE OPERATIONS. THE SECOND SEEDING SHALL BE DONE IMMEDIATELY AFTER THE FIRST AND AT RIGHT ANGLES TO THE FIRST SEEDING AND HIGHLY RAKED INTO THE SOIL. MULCH SEEDED AREAS IMMEDIATELY AFTER SEEDING.

SOIL EROSION AND SEDIMENT CONTROL NARRATIVE

THE PURPOSE OF THIS PROJECT IS TO CONSTRUCT IMPROVMENT TO THE TRAIN WRECK PARK. THESE IMPROVEMENTS INCLUDE A PAVED PARKING LOT, AIRLINE TRAIL, RELOCATION AND INTERPRETIVE AREA IMPROVEMENTS. SITE WORK WILL INCLUDE CONSTRUCTION OF DRAINAGE, PAVING, PAVEMENT MARKINGS, SIGNS, LANDSCAPING, GUIDERAIL, ETC.

ATTENTION SHALL BE GIVEN TO THE INSTALLATION AND MAINTENANCE OF EROSION CONTROL MEASURES. NO ERODED SEDIMENTS SHALL BE PERMITTED TO FLOW INTO EAST THOMPSON ROAD OR DOWNSTREAM CATCHBASINS. IF FIELD CONDITIONS WARRANT IT OR THE TOWN REQUESTS IT, ADDITIONAL E & S CONTROL MEASURES, BEYOND WHAT IS SHOWN ON THE PLAN, SHALL BE INSTALLED.

SEDIMENT AND EROSION CONTROL DEVICES WILL BE INSTALLED AS DETAILED ON THIS SHEET AND CHECKED REGULARLY FOR REPLACEMENT AND AFTER EVERY RAIN FOR REMOVAL OF DEPOSITED MATERIALS. RESPONSIBILITY FOR COMPLIANCE WITH THIS PLAN SHALL BELONG TO THE CONTRACTOR. THE CONTRACTOR SHALL BE THE DESIGNATED ON-SITE AGENT RESPONSIBLE FOR ENSURING TO THE TOWN THAT E & S CONTROL MEASURES ARE STRICTLY ENFORCED.

SEEDING DATES FOR PERMANENT VEGETATION ARE APRIL 1 - JUNE 15 AND AUGUST 15 - SEPTEMBER 15. SEEDING DATES FOR TEMPORARY VEGETATION ARE MARCH 1 - OCTOBER 15. OUTSIDE OF THESE DATES TEMPORARY MULCH CONSISTING OF STRAW OR HAY APPLIED AT THE RATE OF 95 LB/1000 SQUARE FEET SHALL BE USED. HYDROSEEDING WILL BE PERMITTED WHERE SLOPES ARE NO STEEPER THAN 2 TO 1 AND SEEDING RATES WILL BE INCREASED BY 10%.

OPERATIONS AND MAINTENANCE

1. ALL PROPOSED WORK SHALL CONFORM TO "2023 CONNECTICUT GUIDELINES FOR SOIL EROSION AND SEDIMENT CONTROL" BY THE CONNECTICUT COUNCIL OF SOIL AND WATER CONSERVATION AND TOWN REGULATIONS.
2. THE CONTRACTOR IS RESPONSIBLE FOR ENSURING THAT THE GOALS OF THIS EROSION CONTROL PLAN ARE MET BY WHATEVER MEANS ARE NECESSARY. THE CONTRACTOR SHALL PLAN ALL LAND DISTURBING ACTIVITIES IN A MANNER AS TO MINIMIZE THE EXTENT OF DISTURBED AREAS.
3. PRIOR TO CONSTRUCTION OR EXCAVATION, SEDIMENT BARRIERS SHALL BE INSTALLED IN LOCATIONS AS SHOWN ON THE PLAN OR AS REQUIRED BY THE TOWN AND MAINTAINED THROUGHOUT CONSTRUCTION.
4. UPON FINAL GRADING, DISTURBED AREAS SHALL COVERED WITH A MINIMUM OF 6" LOAM AND SEEDED WITH PERENNIAL GRASSES AS SPECIFIED FOR THE PROJECT. IMMEDIATELY AFTER SEEDING, MULCH THE SEEDED AREA, NOT COVERED WITH EROSION CONTROL BLANKET, WITH HAY OR STRAW AT THE RATE OF 2 TONS PER ACRE. SEEDING DATES ARE TO BE BETWEEN APRIL 1 THRU JUNE 15 AND AUGUST 15 THRU OCTOBER 15.
5. DAILY INSPECTIONS SHALL BE MADE OF EROSION AND SEDIMENT CONTROL MEASURES TO INSURE EFFECTIVENESS AND IMMEDIATE CORRECTIVE ACTION SHALL BE TAKEN IF FAILURE OCCURS. ADDITIONAL EROSION CONTROL MEASURES BEYOND WHAT IS SHOWN ON THE PLAN MAY BE NECESSARY.
6. EROSION AND SEDIMENT CONTROL MEASURES SHALL REMAIN IN PLACE UNTIL DISTURBED AREAS HAVE BEEN STABILIZED AND VEGETATIVE COVER HAS BEEN ESTABLISHED, AT WHICH TIME THEY SHALL BE REMOVED.
7. SITE CONTRACTOR SHALL BE RESPONSIBLE FOR IMPLEMENTATION AND MAINTENANCE OF THIS EROSION AND SEDIMENT CONTROL PLAN.

MINIMIZE DISTURBED AREAS

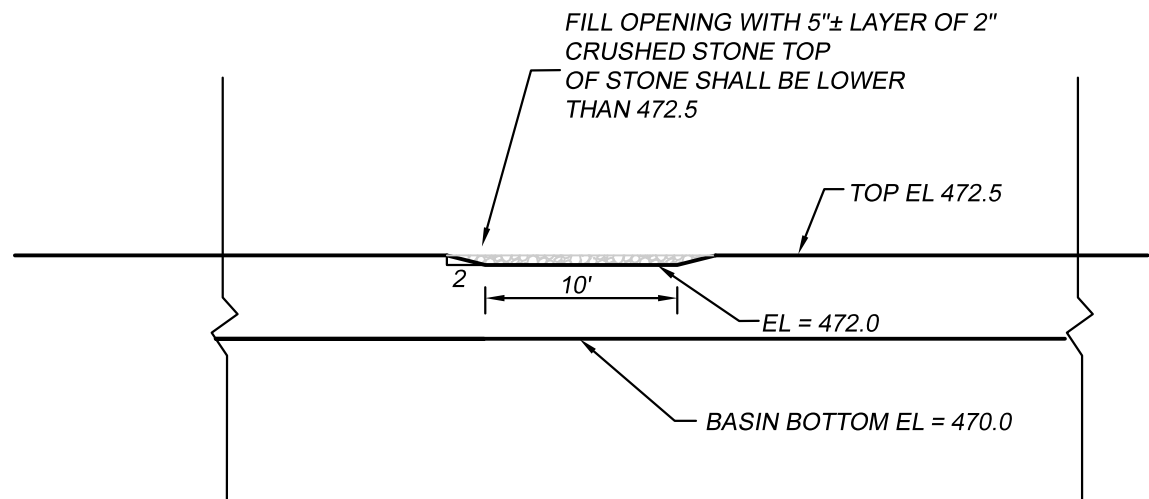
1. KEEP LAND DISTURBANCE TO A MINIMUM - THE MORE LAND THAT IS IN VEGETATIVE COVER, THE MORE SURFACE WATER WILL INFILTRATE INTO THE SOIL, THUS MINIMIZING STORMWATER RUNOFF AND POTENTIAL EROSION. KEEPING LAND DISTURBANCE TO A MINIMUM NOT ONLY INVOLVES MINIMIZING THE EXTENT OF EXPOSURE AT AN ONE TIME, BUT ALSO THE DURATION OF EXPOSURE.
2. PHASE CONSTRUCTION SO THAT AREAS WHICH ARE ACTIVELY BEING DEVELOPED AT ANY ONE TIME ARE MINIMIZED AND ONLY THAT AREA UNDER CONSTRUCTION IS EXPOSED. CLEAR ONLY THOSE AREAS ESSENTIAL FOR CONSTRUCTION.
3. SEQUENCE THE CONSTRUCTION OF STORM DRAINAGE SYSTEMS SO THAT THEY ARE OPERATIONAL AS SOON AS POSSIBLE DURING CONSTRUCTION. ENSURE ALL OUTLETS ARE STABLE BEFORE OUTLETTING STORM DRAINAGE FLOW INTO THEM.
4. SCHEDULE CONSTRUCTION SO THAT FINAL GRADING AND STABILIZATION IS COMPLETED AS SOON AS POSSIBLE.

MANAGING RUNOFF

1. USE DIVERSIONS, STONE DIKES, SILT FENCES AND SIMILAR MEASURES TO BREAK FLOW LINES AND DISSIPATE STORM WATER ENERGY.
2. AVOID DIVERTING ONE DRAINAGE SYSTEM INTO ANOTHER WITHOUT CALCULATING THE POTENTIAL FOR DOWNSTREAM FLOODING OR EROSION.
3. CLEAN RUNOFF SHOULD BE KEPT SEPARATED FROM SEDIMENT LADEN WATER AND SHOULD NOT BE DIRECTED OVER DISTURBED AREAS WITHOUT ADDITIONAL CONTROLS. ADDITIONALLY, PREVENT THE MIXING OF CLEAN OFF-SITE GENERATED RUNOFF WITH SEDIMENT LADEN RUNOFF GENERATED ON-SITE UNTIL AFTER ADEQUATE INFILTRATION OF ON -SITE WATERS HAS OCCURRED.

INTERNAL EROSION CONTROLS

1. DO NOT RELY EXCLUSIVELY ON PERIMETER EROSION CONTROL DEVICES.
2. CONTROL EROSION AND SEDIMENTATION BY INSTALLING INTERNAL EROSION CONTROL IN THE SMALLEST DRAINAGE AREA POSSIBLE.
3. DIRECT RUNOFF FROM SMALL DISTURBED AREAS TO ADJOINING UNDISTURBED VEGETATED AREAS.
4. CONCENTRATED RUNOFF SHOULD BE CONVEYED TO SEDIMENT TRAPS OR BASINS AND STABLE OUTLETS USING RIP RAPPED CHANNELS, STORM DRAINS OR SIMILAR MEASURES.



EMERGENCY SPILLWAY SECTION

N.T.S.

NOTES AND DETAILS
PREPARED FOR
TOWN OF THOMPSON
EAST THOMPSON ROAD - THOMPSON, CT

J&D CIVIL
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401 RAVENELLE ROAD
N. GROSVENORDALE, CT 06255
860-923-2920

DESIGNED: APS
CHECKED: JAB

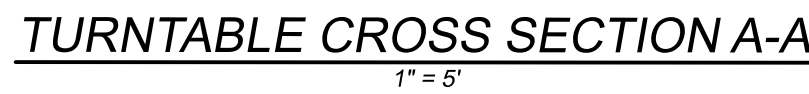
REVISIONS:

JOB NO: 23239

DATE: MARCH 26, 2024

SCALE: 1" = 20'

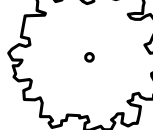


SHEET: 5



PERVIOUS WEED CONTROL LANDSCAPING FABRIC SHALL BE INSTALLED AROUND PLANTS IN PLANTING BEDS WHERE APPROPRIATE. FABRIC SHALL BE COVERED WITH 4" SHREDDED BARK MULCH.



1" = 40' HORIZ.
1" = 4' VERT.

LANDSCAPING SCHEDULE & NOTES		
SYMBOL	SIZE	DESCRIPTION
	2 1/2" - 3"	<u>DECIDUOUS SHADE TREES (ADD ALTERNATE):</u> ACER RUBRUM: RED MAPLE (RED SUNSET OR AUTUMN FLAME OR HYBRIDS WITH SILVER MAPLE - ACER FREEMANII). ACER SACCHARUM: SUGAR MAPLE, FRAXINUS PENNSYLVANICA: GREEN ASH (PATMORE OR CIMMARON). GLEDITSIA TRICANTHOS: HONEYLOCUST (SHADEMASTER OR SKYLINE), PLATANUS X ACERFOLIA: LONDON PLANE TREE (BLOODGOOD OR COLUMBIA)
	3 GALLON	<u>SCREENING TREES AND SHRUBS (BASE BID):</u> ALTERNATE THREE THUJA X PLICATA "JUNIOR GIANT" WITH TWO ARROWWOOD VIBURNUM - VIBURNUM DENTATUM (BLUE MUFFIN, GLITTERS & GLOWS- ADJACENT PLANTS SHOULD BE POLLINATORS) SLIPPE PLANTS 5' O.C. 8' FROM EDGE OF TRAIL
		

NOTES AND DETAILS
PREPARED FOR
TOWN OF THOMPSON
EAST THOMPSON ROAD - THOMPSON, CT

J&D CIVIL ENGINEERS, LLC
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CHECKED: JAB

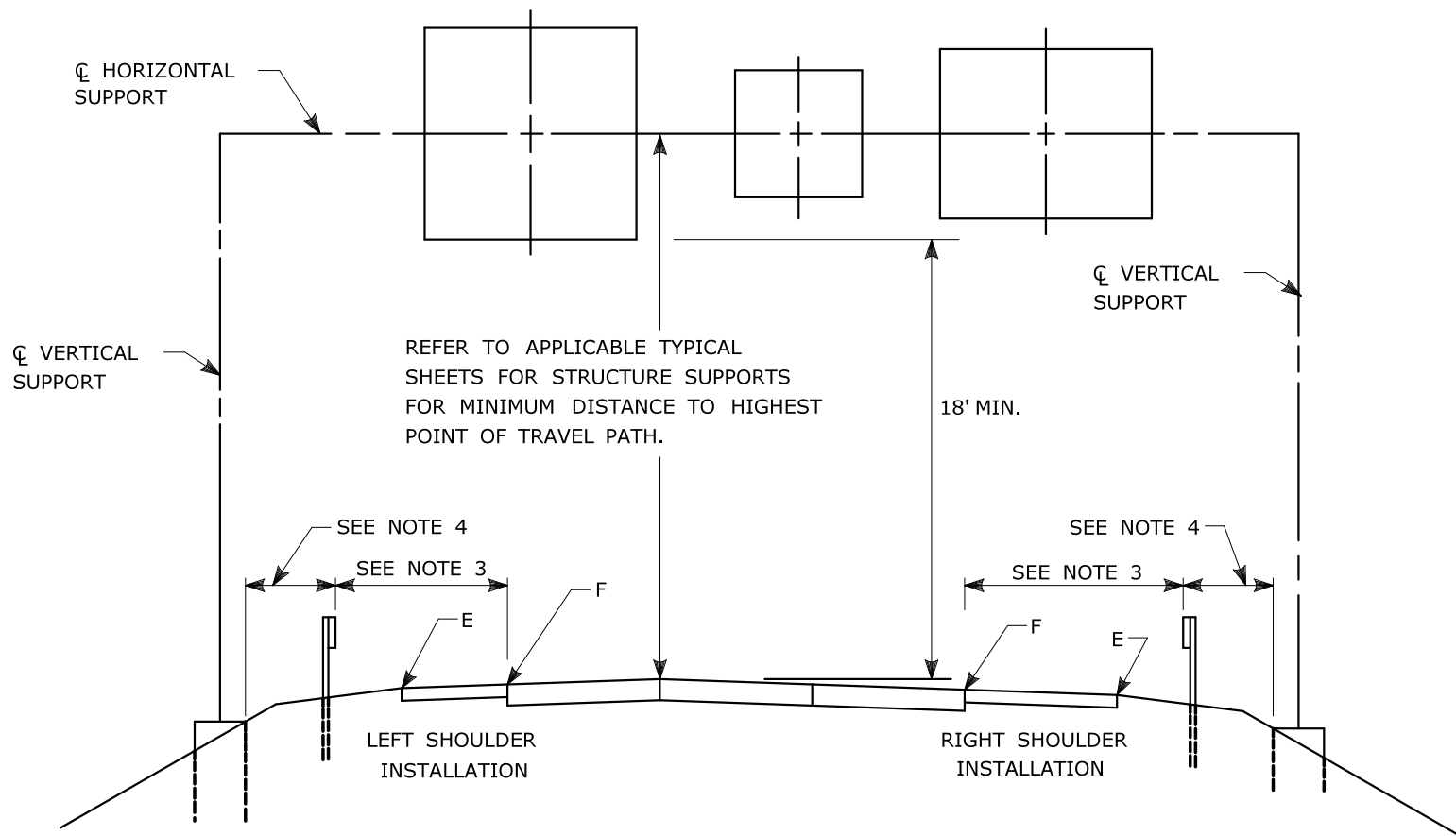
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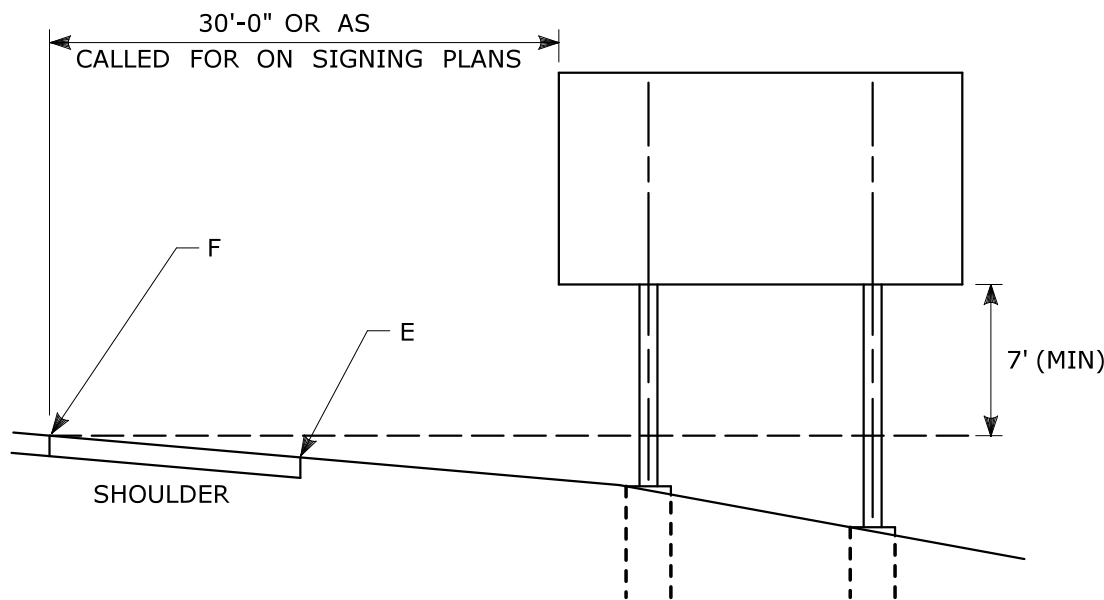
SCALE: 1" = 20'

SHEET: 6



TYPICAL PLACEMENT OF OVERHEAD SIGNS ON SIGN SUPPORTS

- NOTES:
- 1) FOR PLACEMENT OF CANTILEVER SIGN SUPPORT USE APPLICABLE PORTION OF ABOVE DETAIL.
 - 2) BARRIER SYSTEMS MAY BE REQUIRED FOR BOTH SIDES OF SUPPORTS IN MEDIANS.
 - 3) IMPACT PROTECTION SHALL BE PROVIDED FOR THE SIGN SUPPORTS LOCATED WITHIN CLEAR ZONE.
 - 4) SIGN SUPPORT FOUNDATIONS SHALL BE LOCATED OUTSIDE OF BARRIER SYSTEMS DEFLECTION AREA.
 - 5) ALL SIGNS ARE TO BE LEVEL, REGARDLESS OF CAMBER IN SUPPORT.



TYPICAL PLACEMENT OF SIDE MOUNTED SIGNS ON STRUCTURAL STEEL BREAKAWAY SIGN SUPPORTS

- NOTES:
- 1) MIN. VERTICAL CLEARANCE ABOVE SIDEWALKS SHALL BE 7'.
 - 2) WHERE GUIDE RAIL IS USED, THE OFFSET TO THE NEAR EDGE OF SIGN FACE SHALL BE AS SHOWN ELSEWHERE IN THE CONTRACT PLANS.
 - 3) ON INTERSECTING ROADS AT RAMP TERMINI, THE OFFSET TO THE NEAR EDGE OF OF SIGN FACE SHALL BE 6' MIN. FROM POINT "E".
 - 4) IF 30'-0" MIN. CANNOT BE MET, PLEASE CONTACT THE ENGINEER.

FOR MAXIMUM EFFECTIVENESS, POSITION SIDE MOUNTED SIGNS ON STRUCTURAL STEEL BREAKAWAY SIGN SUPPORTS AS FOLLOWS:

ON A TANGENT SECTION, POSITION THE SIGN SO THE VERTICAL AXIS IS PLUMB AND THE HORIZONTAL AXIS IS AT AN ANGLE OF 90° WITH THE TRAFFIC LANE WHICH THE SIGN SERVES. SIGNS LOCATED 30 FT OR MORE FROM THE EDGE OF THE ROAD SHALL BE TURNED APPROXIMATELY 3° TOWARD THE ROAD.

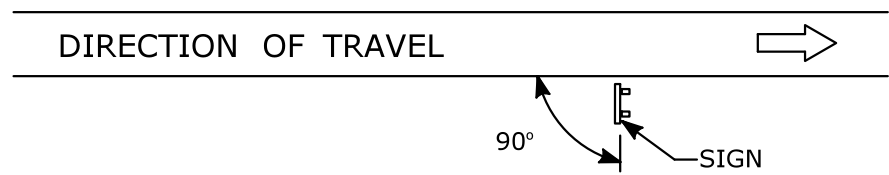


DIAGRAM "A"

ON A HORIZONTAL CURVE SECTION, POSITION THE SIGN SO THE VERTICAL AXIS IS PLUMB AND THE HORIZONTAL AXIS IS AT AN ANGLE OF 90° WITH A STRAIGHT LINE BETWEEN THE SIGN AND THE POINT AT WHICH THE SIGN SHALL BE READ.

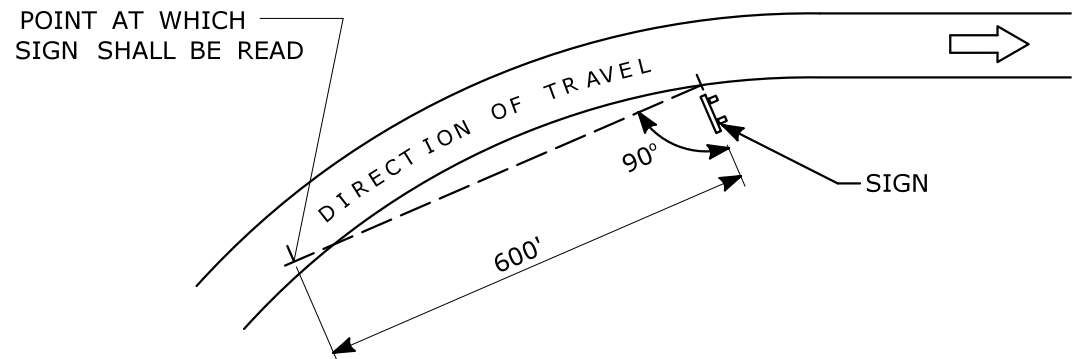
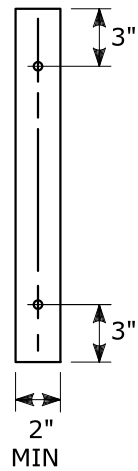


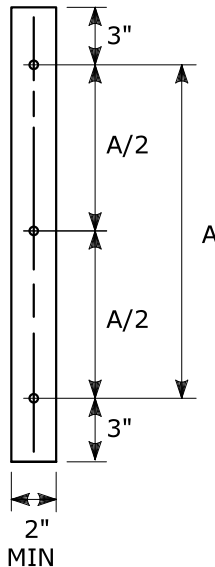
DIAGRAM "B"

**SIGN ORIENTATION DETAILS
FOR SIDE MOUNTED SIGNS ON
STRUCTURAL STEEL BREAKAWAY SIGN SUPPORTS**

RETROREFLECTIVE STRIPS
48" LONG OR LESS:



RETROREFLECTIVE STRIPS
OVER 48" LONG:



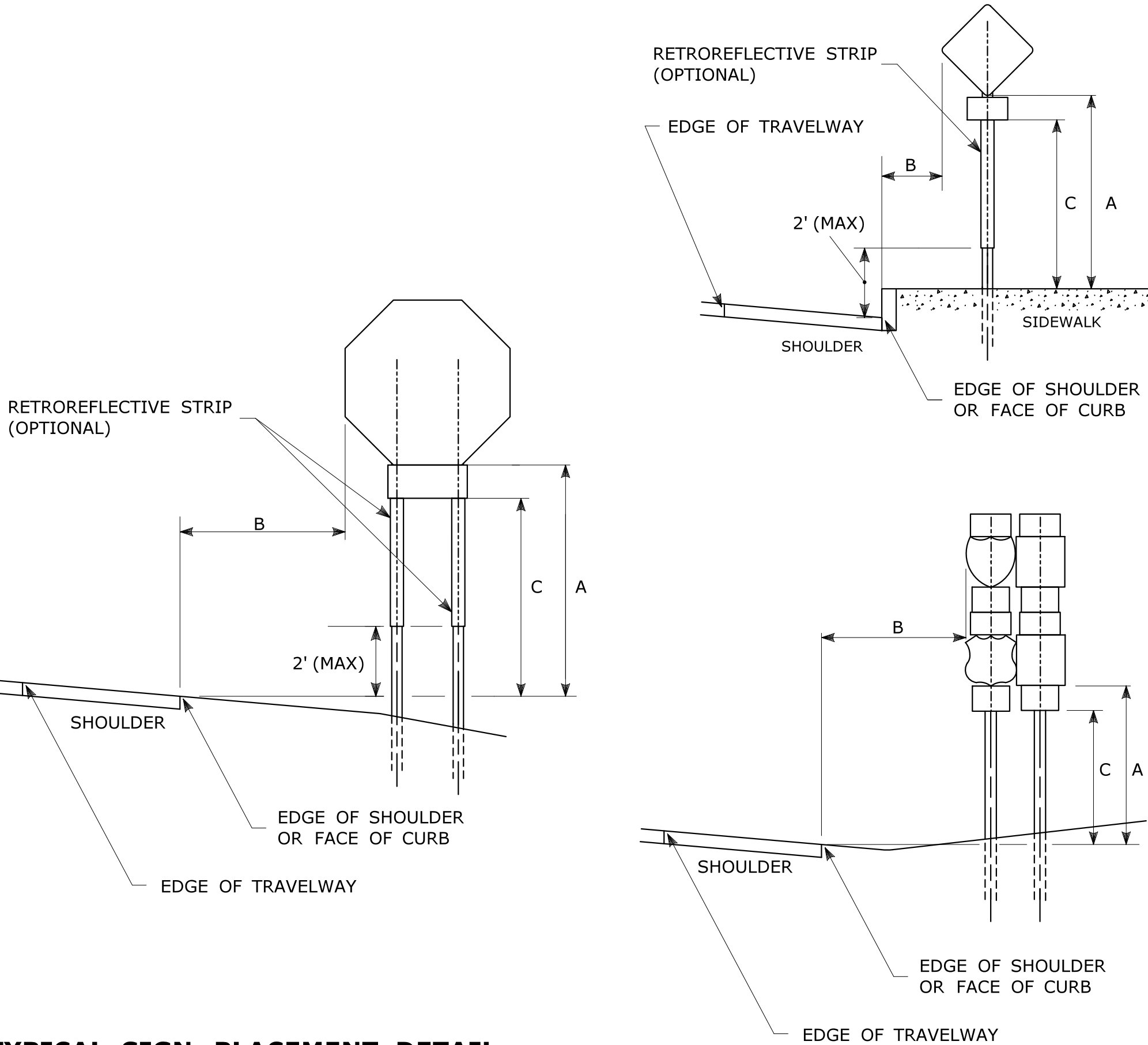
RETROREFLECTIVE STRIP DETAIL

NOTES:

RETROREFLECTIVE STRIPS WHICH ARE 48 IN LONG OR LESS SHALL BE ATTACHED USING 2 BOLTS AND RETROREFLECTIVE STRIPS OVER 48 IN LONG SHALL BE ATTACHED USING 3 BOLTS AS SHOWN ON THE DETAILS ABOVE.

REFER TO STANDARD SHEET No. TR-1208.02 "METAL SIGN POSTS AND SIGN MOUNTING DETAILS" FOR MOUNTING DETAILS.

RETROREFLECTIVE STRIP COLOR SHALL MATCH THE BACKGROUND COLOR OF THE SIGN, EXCEPT THAT THE COLOR OF THE STRIP FOR "YIELD" AND "DO NOT ENTER" SIGNS SHALL BE RED.



TYPICAL SIGN PLACEMENT DETAIL

NOTES:

ALL SIGNS AND SHIELDS ON DIRECTIONAL ASSEMBLIES SHALL ABUT VERTICALLY.

REFER TO STANDARD SHEET No. TR-1208.02 "METAL SIGN POSTS AND SIGN MOUNTING DETAILS" FOR SIGN POSTS AND SIGN MOUNTING.

IF A RETFOREFLECTIVE STRIP IS USED ON SIGN SUPPORT, IT SHALL BE PLACED FOR THE FULL LENGTH OF THE SUPPORT FROM THE BOTTOM OF THE SIGN TO WITHIN 2 FT ABOVE THE EDGE OF THE ROADWAY.

PARKING SIGNS TYPICALLY USE 45° MOUNTING BRACKET.

DIM."A" MIN SIGN HEIGHT	DIM."B" MIN LATERAL OFFSET ①	DIM."C" MIN PLAQUE HEIGHT ①	ASSEMBLY LOCATION
7' ②	6' 12' ③	5'	SIGNS ON FREEWAYS AND EXPRESSWAYS EXCEPT CHEVRON ALIGNMENT SIGNS, ONE-DIRECTION LARGE ARROW SIGNS, DO NOT ENTER SIGNS, AND WRONG WAY SIGNS
5'	2'	4'	• SIGNS IN RURAL AREAS • DO NOT ENTER AND WRONG WAY SIGNS ALONG EXIT RAMP • DO NOT ENTER AND WRONG WAY SIGNS ON LIMITED ACCESS HIGHWAYS
5'	2'	N/A	• CHEVRON ALIGNMENT SIGNS LOCATED ON FREEWAYS, EXPRESSWAYS, RAMP, AND IN RURAL AREAS • ONE-DIRECTION LARGE ARROW SIGNS LOCATED ON FREEWAYS, EXPRESSWAYS, RAMP, AND IN RURAL AREAS
4'	6' 12' ③	N/A	INCIDENT MANAGEMENT SIGNS AND MILE POST MARKER ASSEMBLIES LOCATED ON FREEWAYS AND EXPRESSWAYS
4'	2'	4'	CENTRAL ISLANDS OF ROUNDABOUTS
7'	2' ④	6'	BUSINESS & RESIDENTIAL AREAS WHERE PARKING OR OTHER OBSTRUCTIONS LIMIT VISIBILITY
7'	2' ④	7'	SIDEWALKS ⑤


① OR AS DIRECTED BY THE ENGINEER

② 8 FT MINIMUM HEIGHT REQUIRED IF A SUPPLEMENTAL PLAQUE IS SUBMOUNTED BELOW THE MAJOR SIGN.

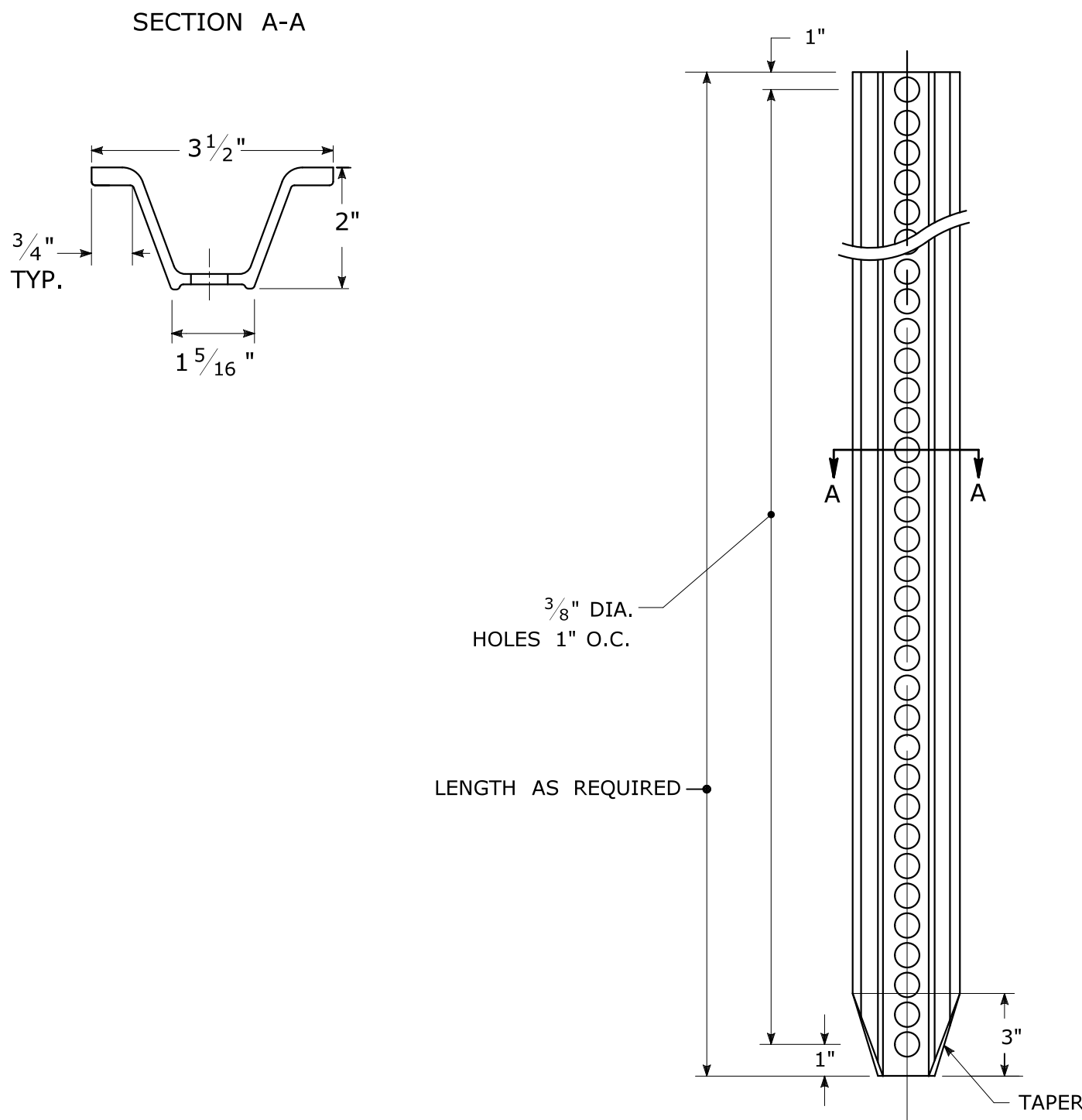
③ 6 FT FROM EDGE OF SHOULDER, WHEN SHOULDER IS OVER 6 FT WIDE
12 FT FROM EDGE OF TRAVELWAY, WHEN SHOULDER IS LESS THAN 6 FT WIDE.

④ A LATERAL OFFSET OF AT LEAST 1 FT FROM THE FACE OF THE CURB MAY BE USED WHERE SIDEWALK WIDTH IS LIMITED OR WHERE EXISTING UTILITY POLES ARE CLOSE TO THE CURB.

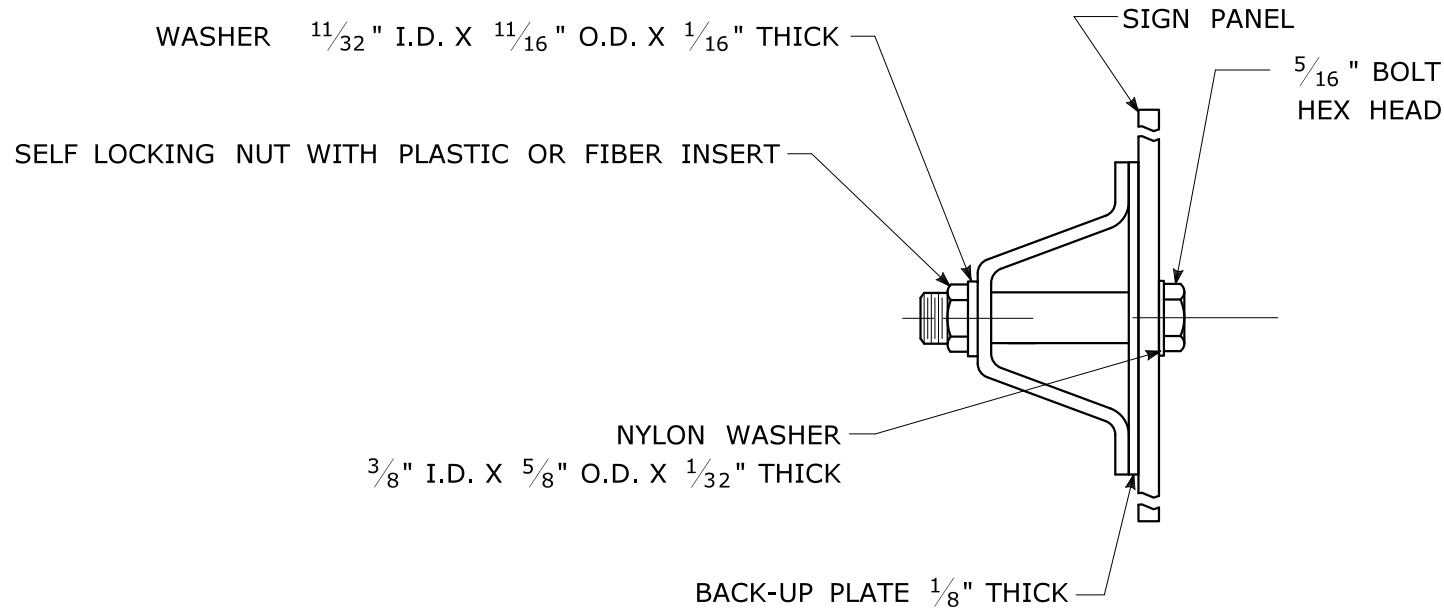
⑤ A CLEAR PATH OF NOT LESS THAN 4 FT SHALL BE PROVIDED IN SIDEWALK AREAS.

			THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.		 STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION	FILENAME: TR_1208_01_1_2018.dgn MODEL: TR-1208_01	SUBMITTED BY: NAME/DATE/TIME: APPROVED BY: NAME/DATE/TIME: 	CTDOT STANDARD SHEET OFFICE OF ENGINEERING	STANDARD SHEET TITLE: SIGN PLACEMENT AND RETROREFLECTIVE STRIP DETAILS	STANDARD SHEET NO.: TR-1208_01
3	8-2018	INCLUDED INCIDENT MANAGEMENT AND MILE MARKER SIGNS.								
2	4-2017	MINOR REVISIONS.								
1	2-2011	MINOR REVISIONS.								
REV.	DATE	REVISION DESCRIPTION		Plotted Date: 8/10/2018	NOT TO SCALE					

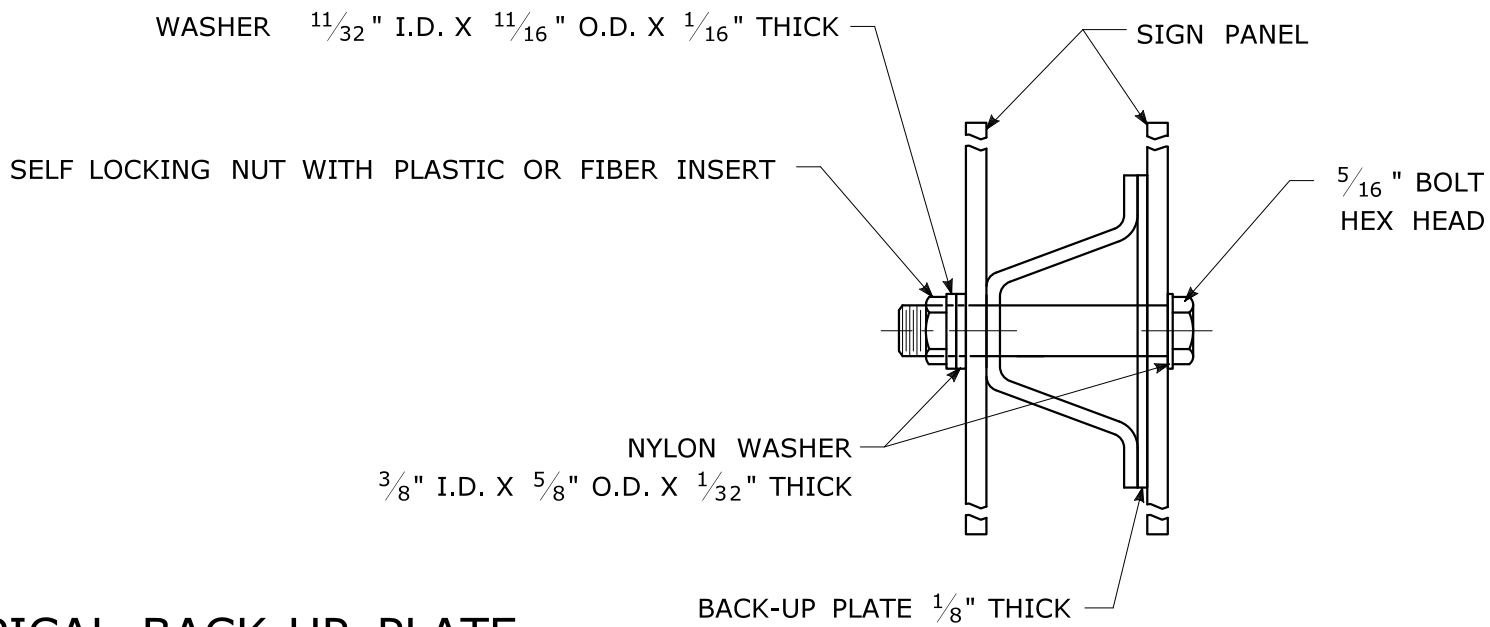
TYPICAL METAL SIGN POSTS



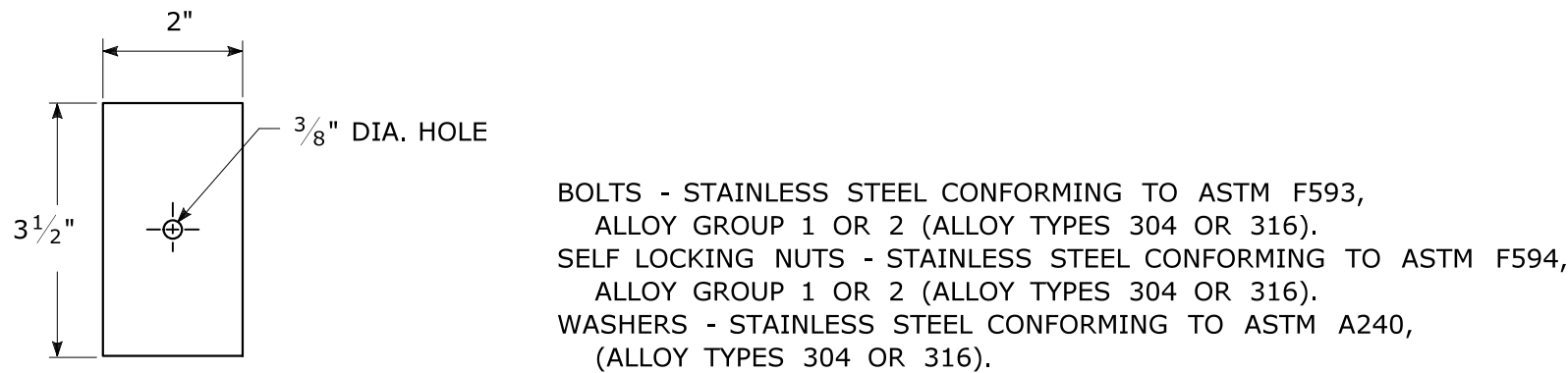
TYPICAL SIGN PANEL ATTACHMENT



TYPICAL BACK TO BACK SIGN PANEL ATTACHMENT

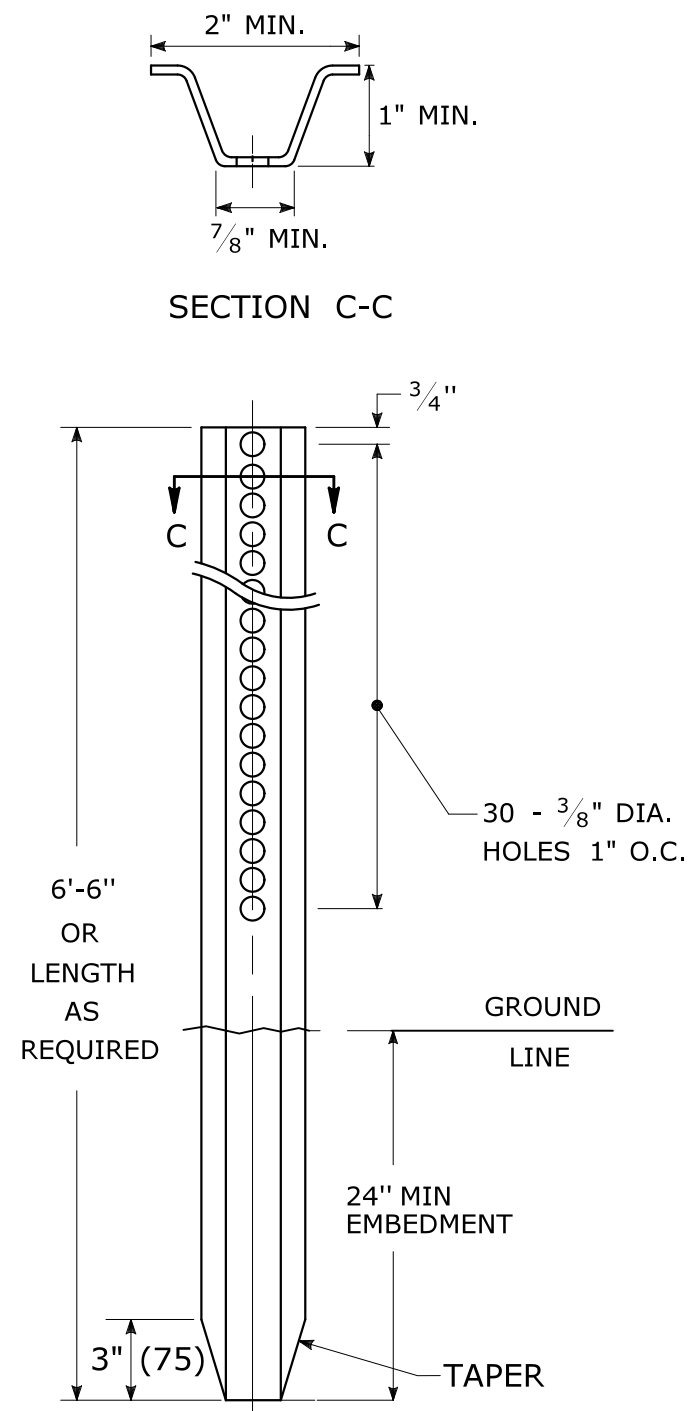


TYPICAL BACK-UP PLATE



METAL DELINEATOR POST

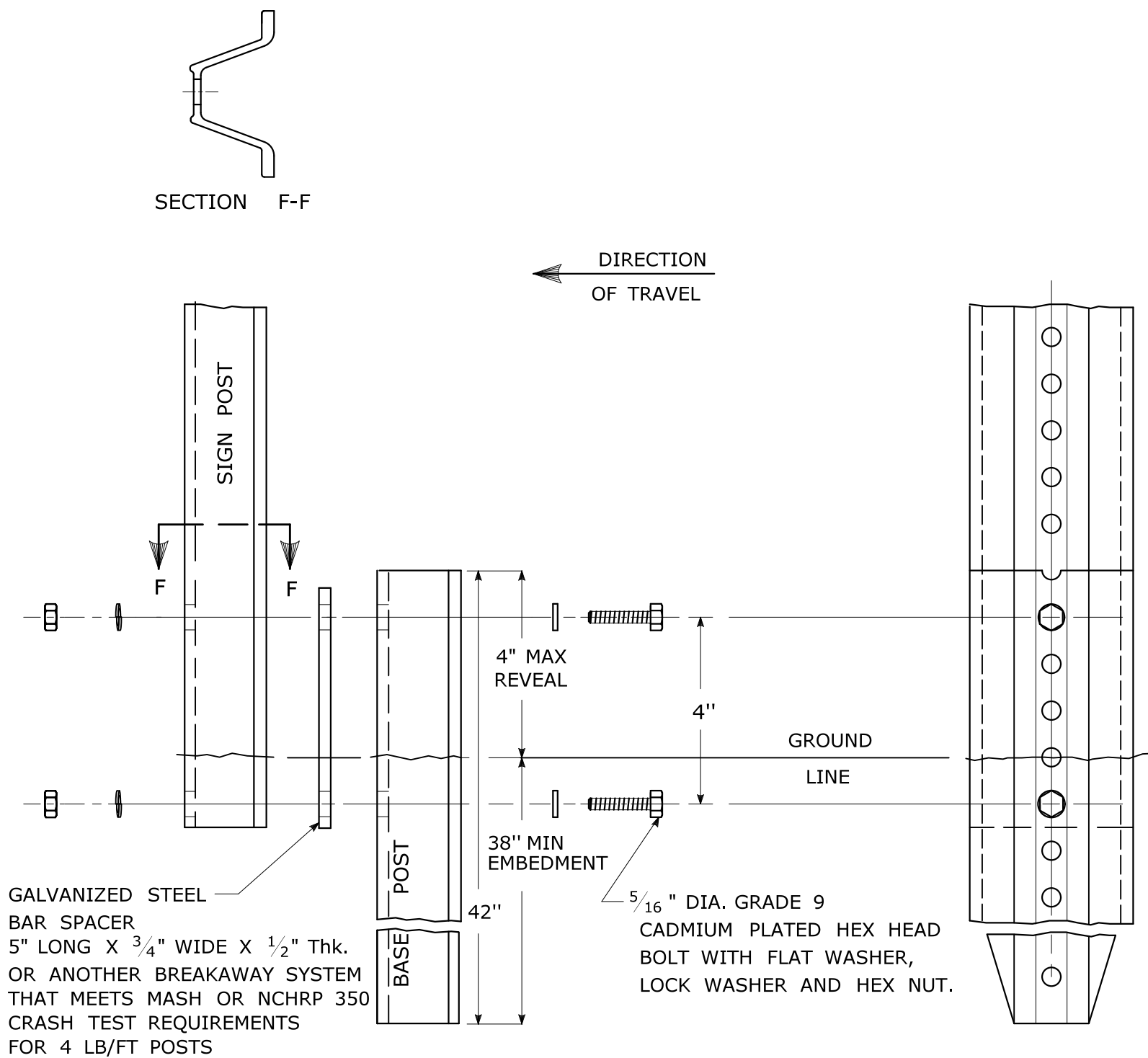
WT./FT. = 1.12 LBS./FT. MIN.



GENERAL NOTES:

1. STEEL FOR DELINEATOR POSTS SHALL BE ASTM A36 STEEL. STEEL FOR ALL OTHER POSTS SHALL CONFORM TO THE MECHANICAL REQUIREMENTS OF ASTM A 499 GRADE 80 AND TO THE CHEMICAL REQUIREMENTS OF ASTM A1 CARBON STEEL TEE RAIL HAVING NOMINAL WEIGHT (MASS) OF 91 LBS. OR GREATER PER LINEAR YARD.
2. AFTER FABRICATION, ALL STEEL POSTS, STRAPS AND PLATES SHALL BE GALVANIZED TO MEET THE REQUIREMENTS OF ASTM A123.
3. WASHERS FOR BREAKAWAY INSTALLATIONS SHALL MEET ASTM F436, TYPE 1.
4. SPACER BAR FOR BREAKAWAY INSTALLATION SHALL CONFORM TO THE MECHANICAL REQUIREMENTS OF ASTM A36.
5. ALL BOLTS, NUTS, AND WASHERS FOR BREAKAWAY INSTALLATIONS SHALL BE GALVANIZED TO MEET THE REQUIREMENTS OF ASTM A153.
6. ALL SIGN POSTS SHALL HAVE BREAKAWAY FEATURES THAT MEET AASHTO REQUIREMENTS CONTAINED IN THE CURRENT "STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES AND TRAFFIC SIGNALS." THE BREAKAWAY FEATURES SHALL BE STRUCTURALLY ADEQUATE TO CARRY THE SIGNS SHOWN IN THE PLANS AT 60 mph WIND LOADINGS. INSTALLATIONS SHALL BE IN ACCORDANCE WITH MANUFACTURERS RECOMMENDATIONS.
7. SIGN POSTS SHALL BE 4 LBS./FT.

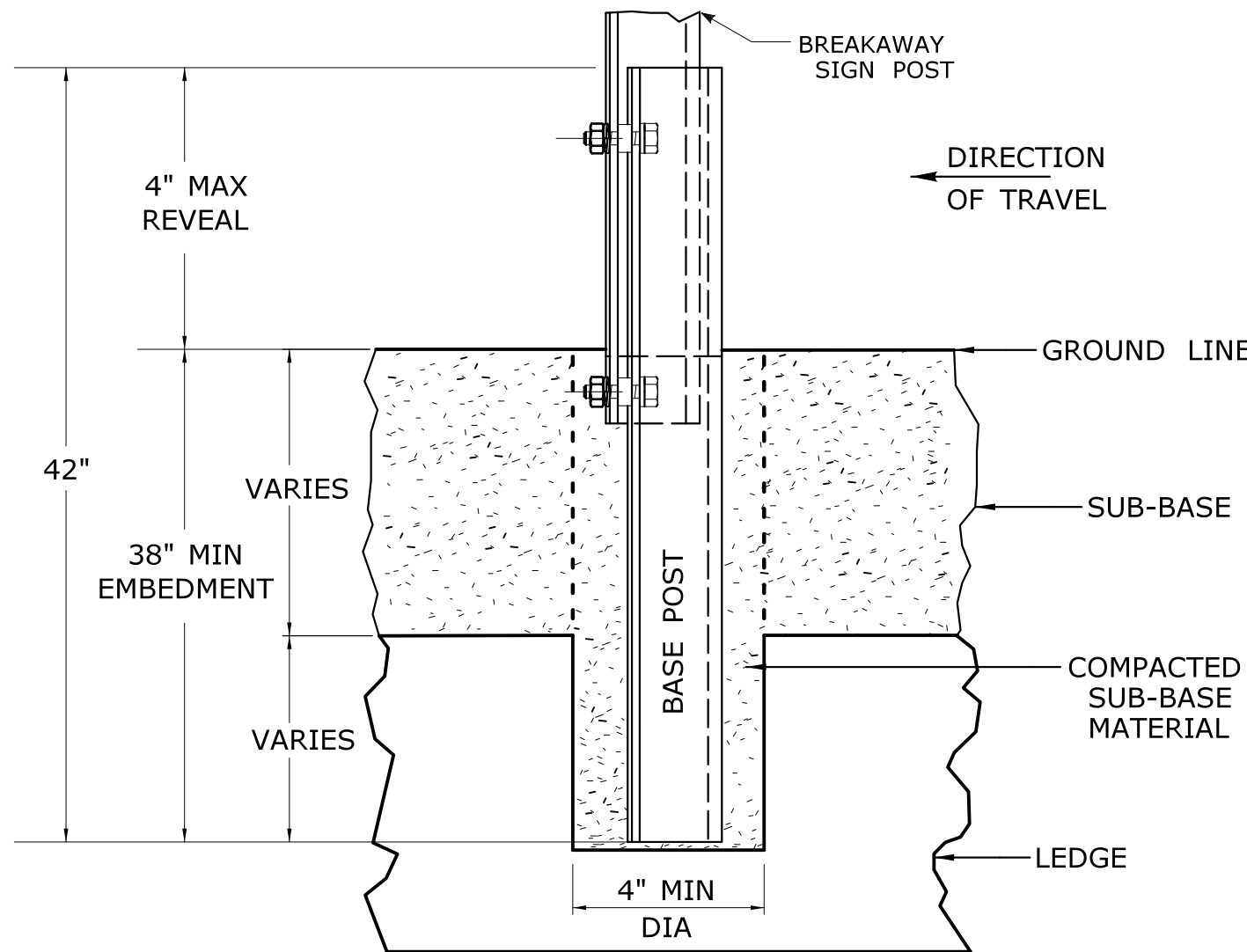
BREAKAWAY INSTALLATION
FOR 4 LBS./FT. POSTS



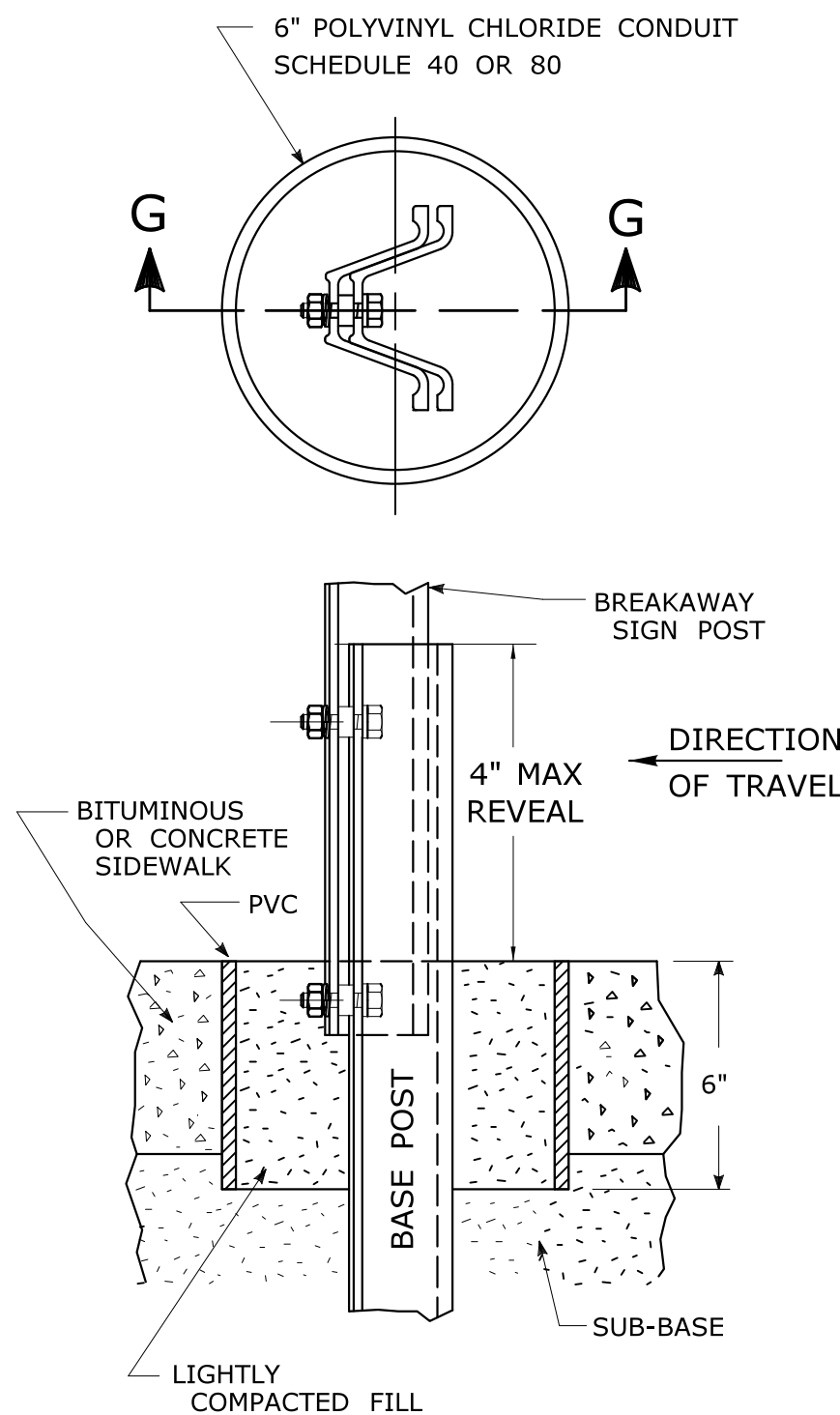
TYPICAL SIGN POST INSTALLATION IN LEDGE

LEDGE SHALL BE REMOVED TO DRIVE THE BASE POST TO A DEPTH OF 38".

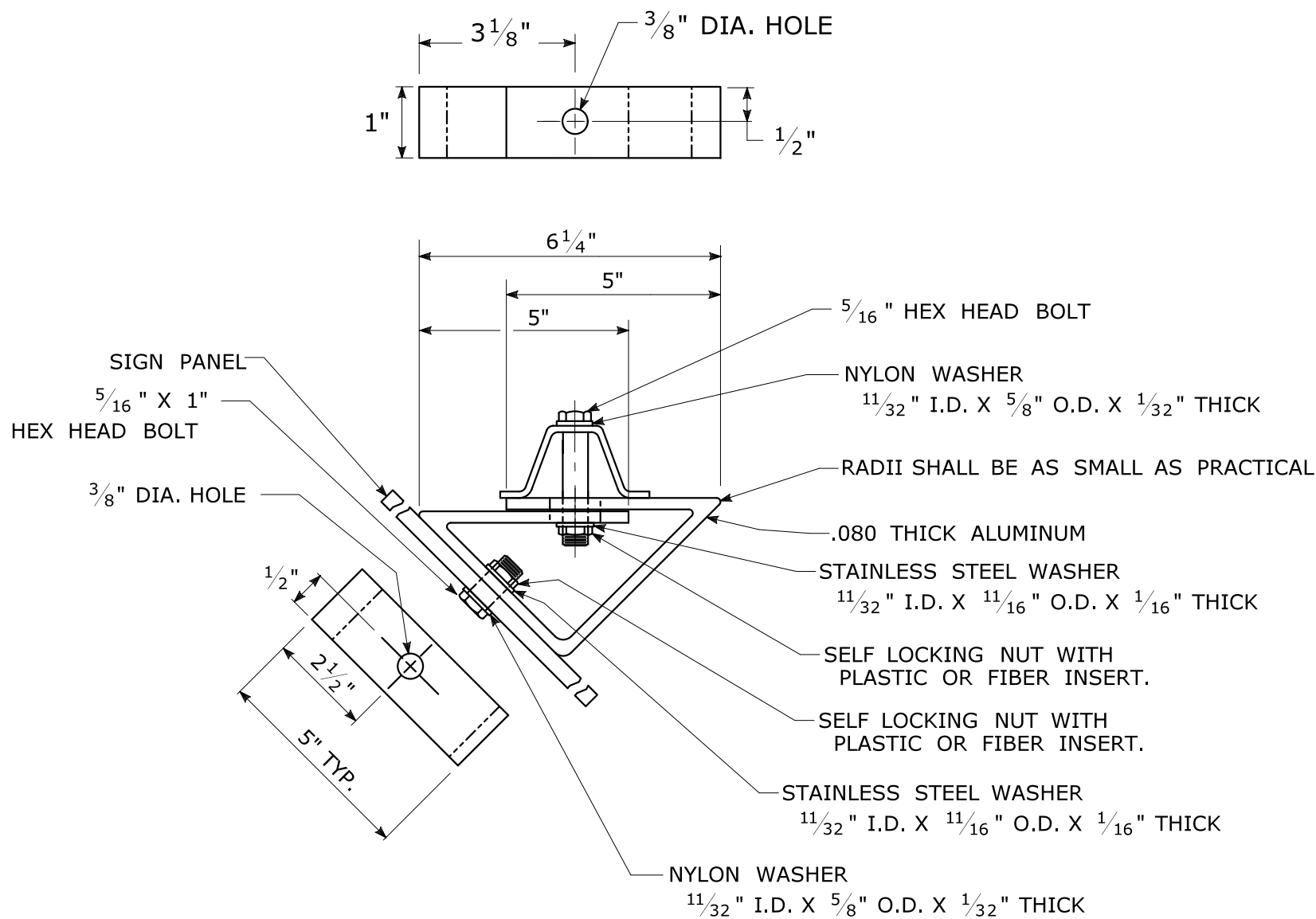
HOLE SHALL BE FILLED WITH SUB-BASE MATERIAL AND COMPACTED WITH A TAMPING BAR, OR TECHNIQUE APPROVED BY THE ENGINEER, PRIOR TO BASE POST INSTALLATION.



TYPICAL SLEEVE
FOR PAVED AREAS



45° MOUNTING BRACKET
FOR INSTALLATION OF PARKING SIGNS

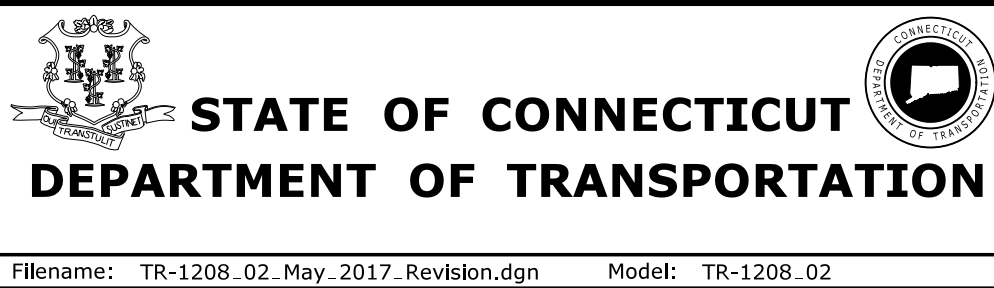


2	6-2017	SIGN POST REVISIONS.
1	2-2011	MINOR REVISIONS.
REV.	DATE	REVISION DESCRIPTION

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Plotted Date: 6/6/2017

NOT TO SCALE



SUBMITTED BY:	NAME/DATE/TIME:
APPROVED BY:	NAME/DATE/TIME:

CTDOT
STANDARD SHEET

OFFICE OF ENGINEERING

STANDARD SHEET TITLE:	GUIDE SHEET NO.:
METAL SIGN POSTS AND SIGN MOUNTING DETAILS	TR-1208_02

